

NOTES:

- (a) 7.5% PREFERRED SLOPE.
- (b) MAXIMUM SLOPE TRANSITION LENGTH MEASURED ALONG THE FACE OF CURB SHALL BE 15 FEET. A TRANSITION SLOPE GREATER THAN 9.5% IS ONLY ACCEPTABLE WHEN THE TRANSITION STONE REACHES A MAXIMUM LENGTH OF 15 FEET.
- (c) THE LANDING CROSS SLOPE (PARALLEL TO THE CURB LINE) AND THE GUTTER RUNNING SLOPE SHALL BE A MAXIMUM OF 1.5% AT APPROACHES UNDER STOP OR YIELD CONTROL. THE SLOPE MAY EXCEED 1.5% MAXIMUM UNDER THE FOLLOWING CONDITIONS:
 - SIGNAL OR PEDESTRIAN HYBRID BEACON= 4.5% MAX.
 - UNCONTROLLED = 4.5% MAX.
 - MIDBLOCK = NO GREATER THAN STREET GRADE
 - ROUNDABOUT = NO GREATER THAN STREET GRADE
- CASE 1 PEDESTRIAN CURB RAMP SHOULD BE ALIGNED PARALLEL TO THE CROSSWALK DIRECTION AS FEASIBLE.
- ± 0.5% MAX. CONSTRUCTION TOLERANCE PERMITTED ON ALL SLOPES.
- CORNER RADII SHOULD BE DESIGNED TO THE MINIMUM DIMENSIONS NECESSARY FOR THE DESIGN VEHICLE.



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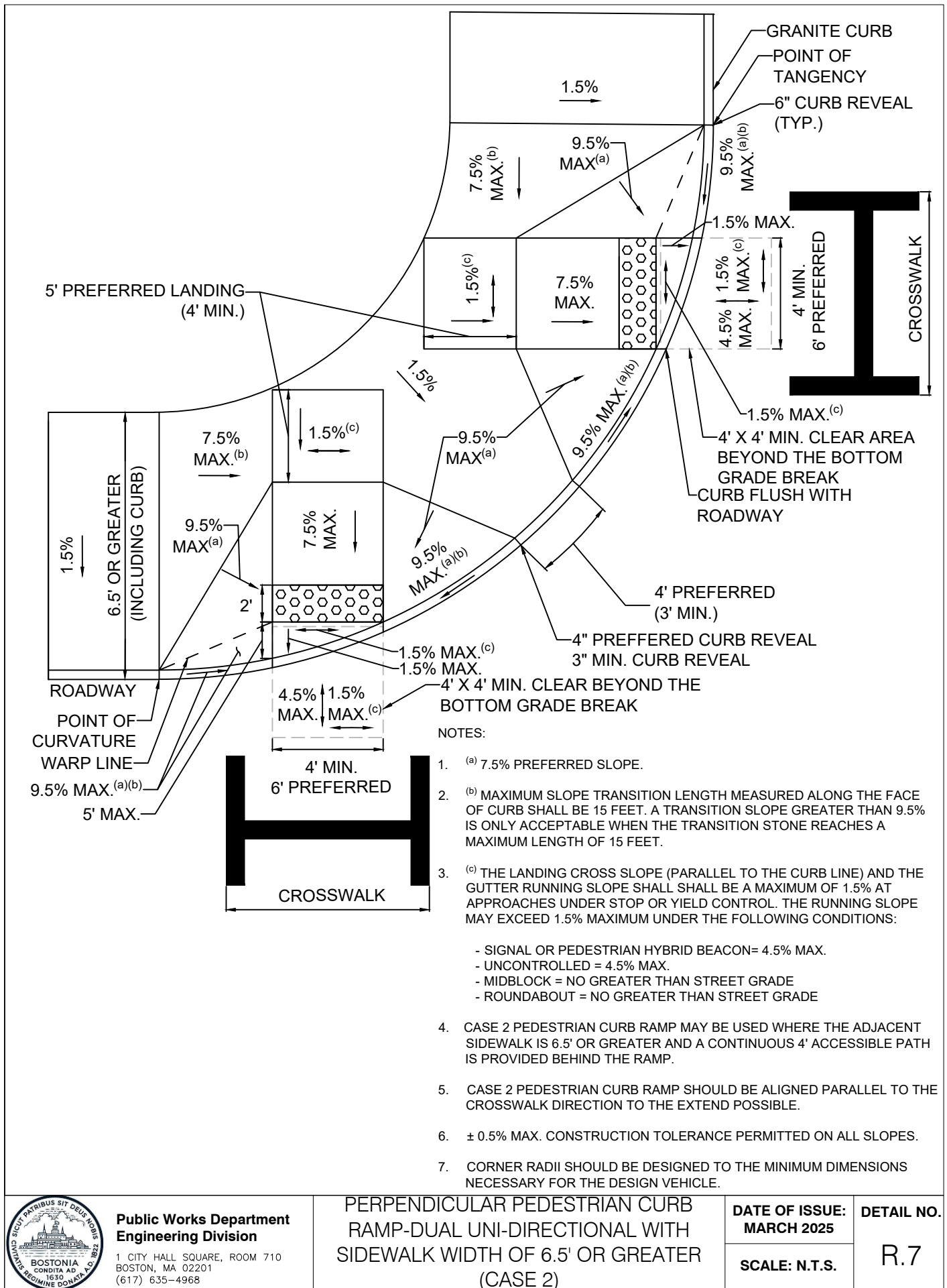
**PERPENDICULAR PEDESTRIAN CURB
RAMP-DUAL UNI-DIRECTIONAL WITH
SIDEWALK WIDTH OF 6.5' OR GREATER
(CASE 1)**

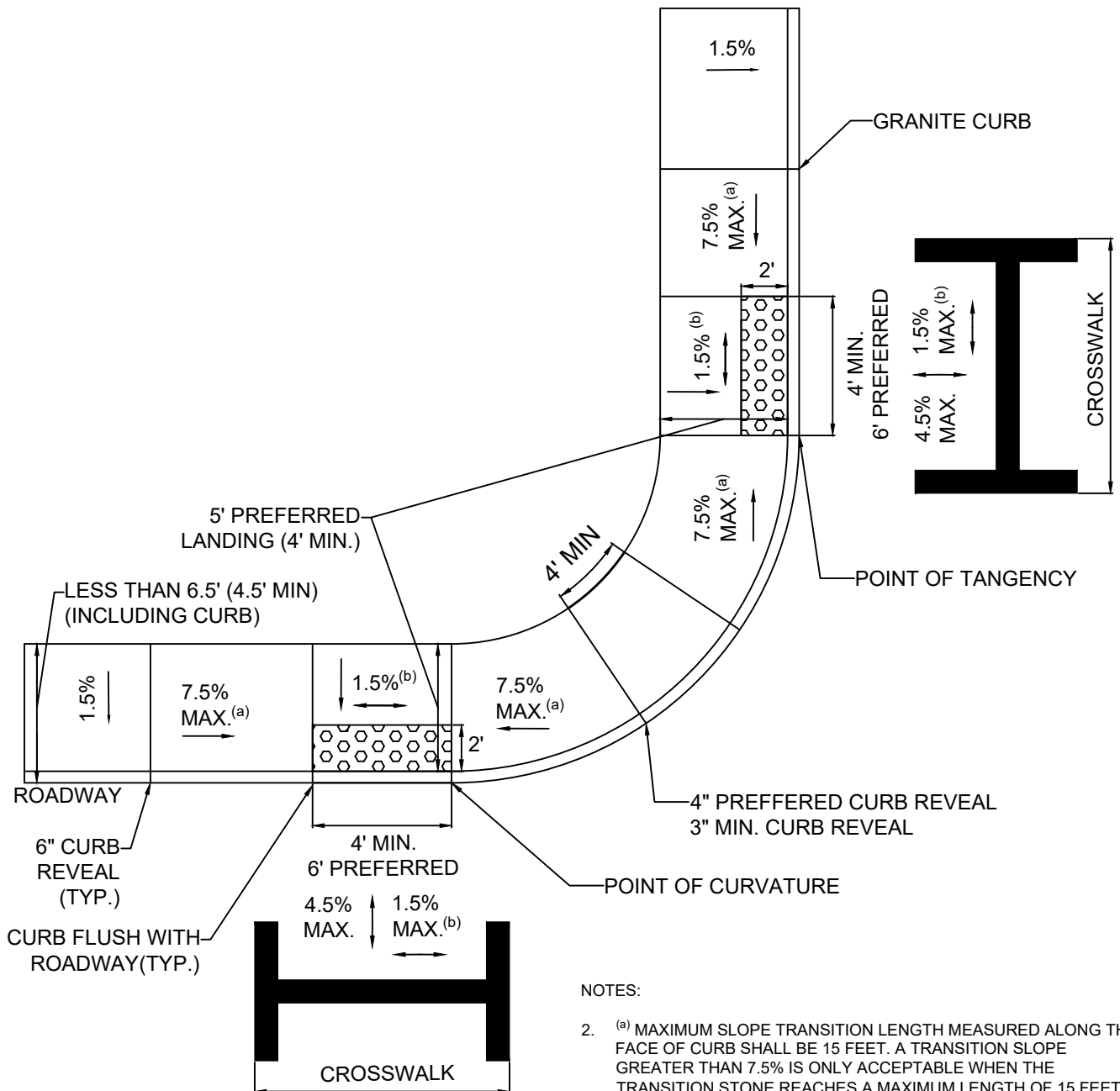
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DETAIL NO.

R.6





NOTES:

2. (a) MAXIMUM SLOPE TRANSITION LENGTH MEASURED ALONG THE FACE OF CURB SHALL BE 15 FEET. A TRANSITION SLOPE GREATER THAN 7.5% IS ONLY ACCEPTABLE WHEN THE TRANSITION STONE REACHES A MAXIMUM LENGTH OF 15 FEET.
3. (b) THE LANDING CROSS SLOPE (PARALLEL TO THE CURB LINE) AND THE GUTTER RUNNING SLOPE SHALL BE A MAXIMUM OF 1.5% AT APPROACHES UNDER STOP OR YIELD CONTROL. THE RUNNING SLOPE MAY EXCEED 1.5% MAXIMUM UNDER THE FOLLOWING CONDITIONS:
 - SIGNAL OR PEDESTRIAN HYBRID BEACON= 4.5% MAX.
 - UNCONTROLLED = 4.5% MAX.
 - MIDBLOCK = NO GREATER THAN STREET GRADE
 - ROUNDABOUT = NO GREATER THAN STREET GRADE
3. $\pm 0.5\%$ MAX. CONSTRUCTION TOLERANCE PERMITTED ON ALL SLOPES.
4. CORNER RADII SHOULD BE DESIGNED TO THE MINIMUM DIMENSIONS NECESSARY FOR THE DESIGN VEHICLE.



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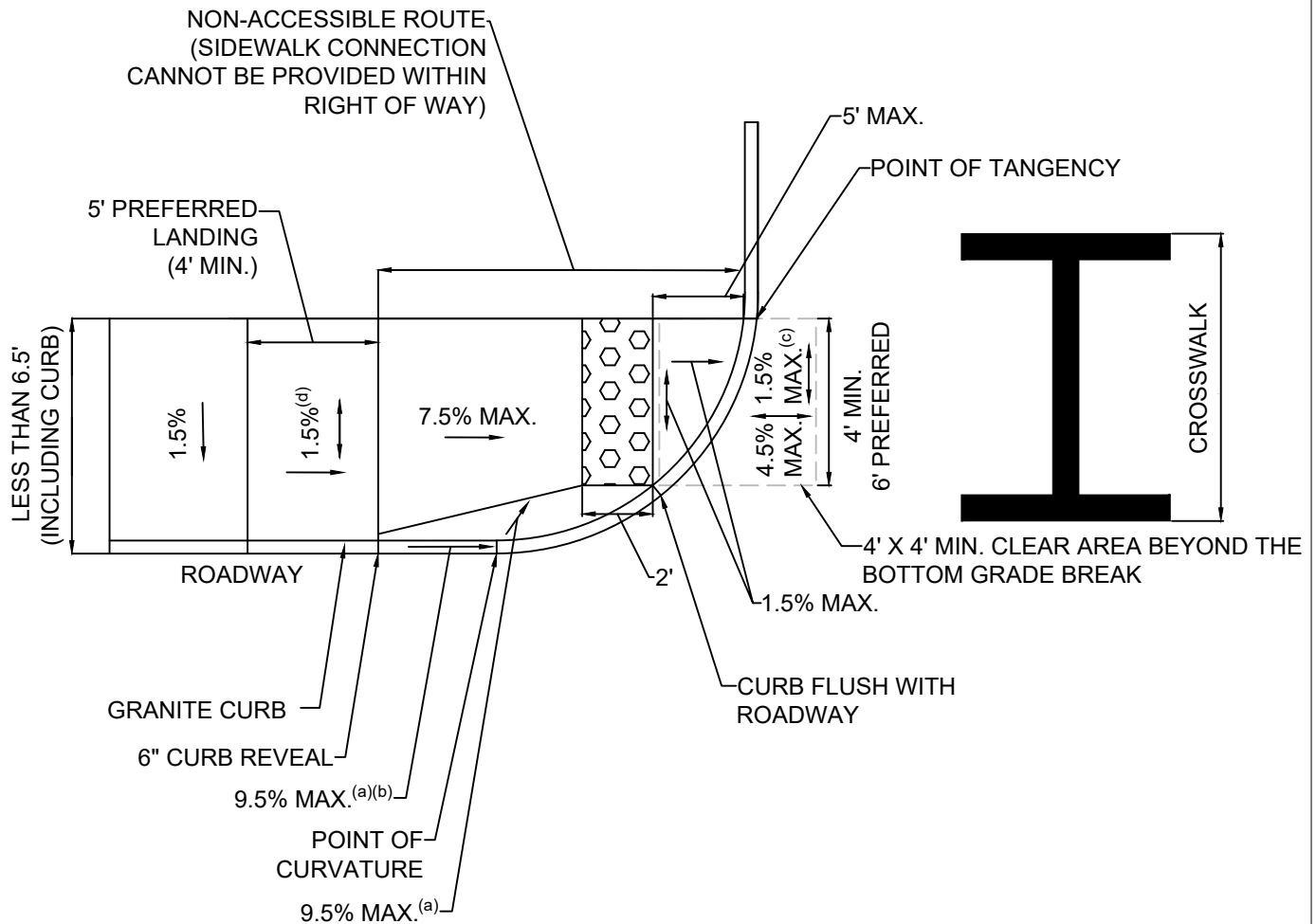
PARALLEL PEDESTRIAN CURB RAMP-DUAL UNI-DIRECTIONAL WITH SIDEWALK WIDTH LESS THAN 6.5'

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DETAIL NO.

R.8



NOTES:

1. (a) 7.5% PREFERRED SLOPE.
2. (b) MAXIMUM SLOPE TRANSITION LENGTH MEASURED ALONG THE FACE OF CURB SHALL BE 15 FEET. A TRANSITION SLOPE GREATER THAN 9.5% IS ONLY ACCEPTABLE WHEN THE TRANSITION STONE REACHES A MAXIMUM LENGTH OF 15 FEET.
2. (c) THE LANDING CROSS SLOPE (PARALLEL TO THE CURB LINE) AND THE GUTTER RUNNING SLOPE SHALL BE A MAXIMUM OF 1.5% AT APPROACHES UNDER STOP OR YIELD CONTROL. THE RUNNING SLOPE MAY EXCEED 1.5% MAXIMUM UNDER THE FOLLOWING CONDITIONS:
 - SIGNAL OR PEDESTRIAN HYBRID BEACON= 4.5% MAX.
 - UNCONTROLLED = 4.5% MAX.
 - MIDBLOCK = NO GREATER THAN STREET GRADE
 - ROUNDABOUT = NO GREATER THAN STREET GRADE
3. (d) THE LANDING RUNNING SLOPE (PARALLEL TO THE CURB LINE) CAN EXCEED 1.5% TO MATCH THE EXISTING SIDEWALK RUNNING SLOPE. THE LANDING CROSS SLOPE (PERPENDICULAR TO THE CURB LINE) SHALL BE A MAXIMUM OF 1.5%.
4. $\pm 0.5\%$ MAX. CONSTRUCTION TOLERANCE PERMITTED ON ALL SLOPES.
5. CORNER RADII SHOULD BE DESIGNED TO THE MINIMUM DIMENSIONS NECESSARY FOR THE DESIGN VEHICLE.



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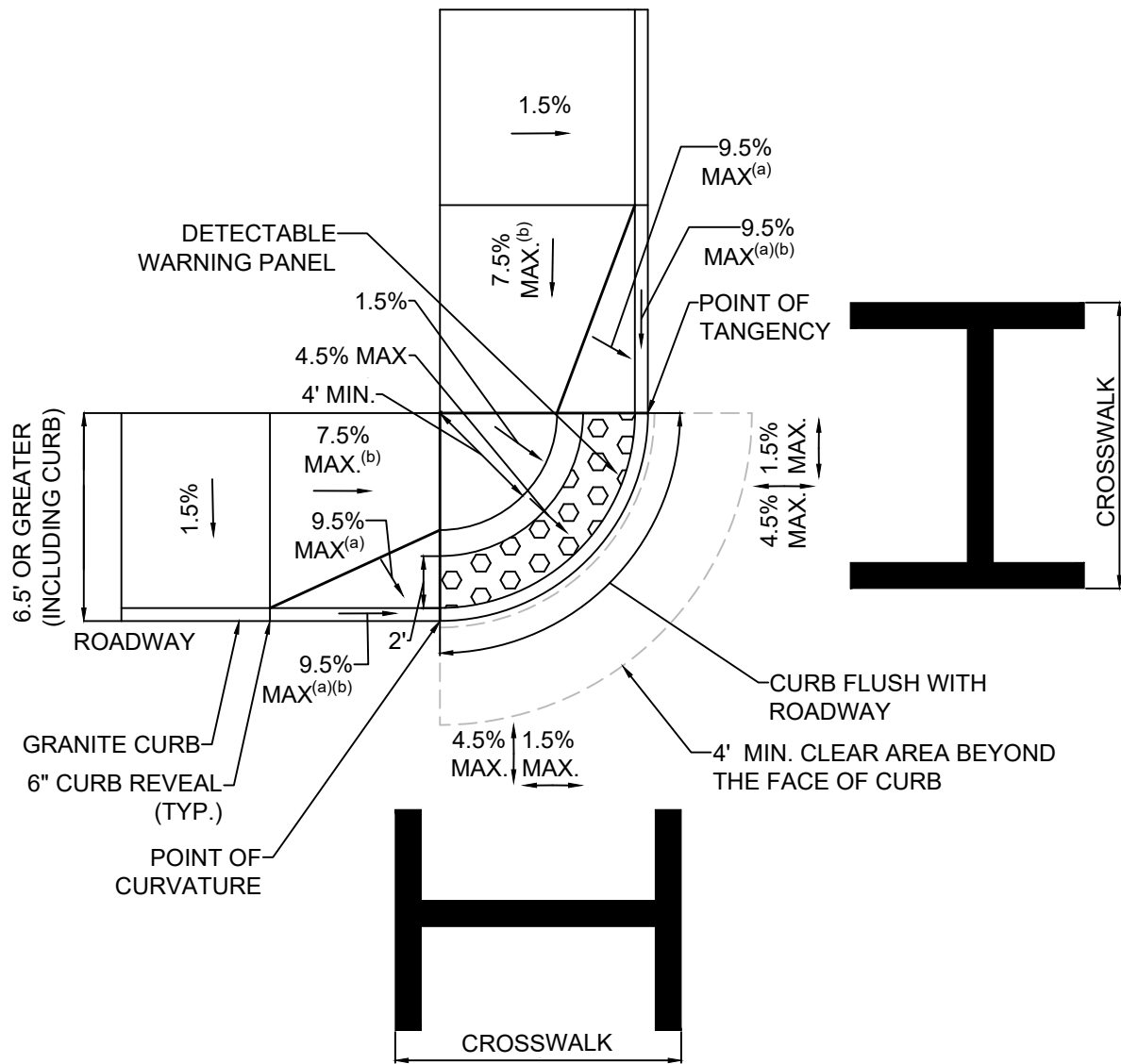
PEDESTRIAN CURB RAMP FOR ONE CONTINUOUS DIRECTION TRAVEL WITH SIDEWALK WIDTH LESS THAN 6.5'

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R.10



NOTES:

- CASE 1 - BLENDED TRANSITIONS SHALL ONLY BE PERMITTED FOR RETROFITTING PROJECTS WHERE EXISTING ROADWAY CONSTRAINTS MAKE UNI-DIRECTIONAL COMPLIANT RAMPS INFEASIBLE.
- ^(a) 7.5% PREFERRED SLOPE.
- ^(b) MAXIMUM SLOPE TRANSITION LENGTH MEASURED ALONG THE FACE OF CURB SHALL BE 15 FEET. A TRANSITION SLOPE GREATER THAN 9.5% IS ONLY ACCEPTABLE WHEN THE TRANSITION STONE REACHES A MAXIMUM LENGTH OF 15 FEET.
- ± 0.5% MAX. CONSTRUCTION TOLERANCE PERMITTED ON ALL SLOPES.
- CORNER RADII SHOULD BE DESIGNED TO THE MINIMUM DIMENSIONS NECESSARY FOR THE DESIGN VEHICLE.



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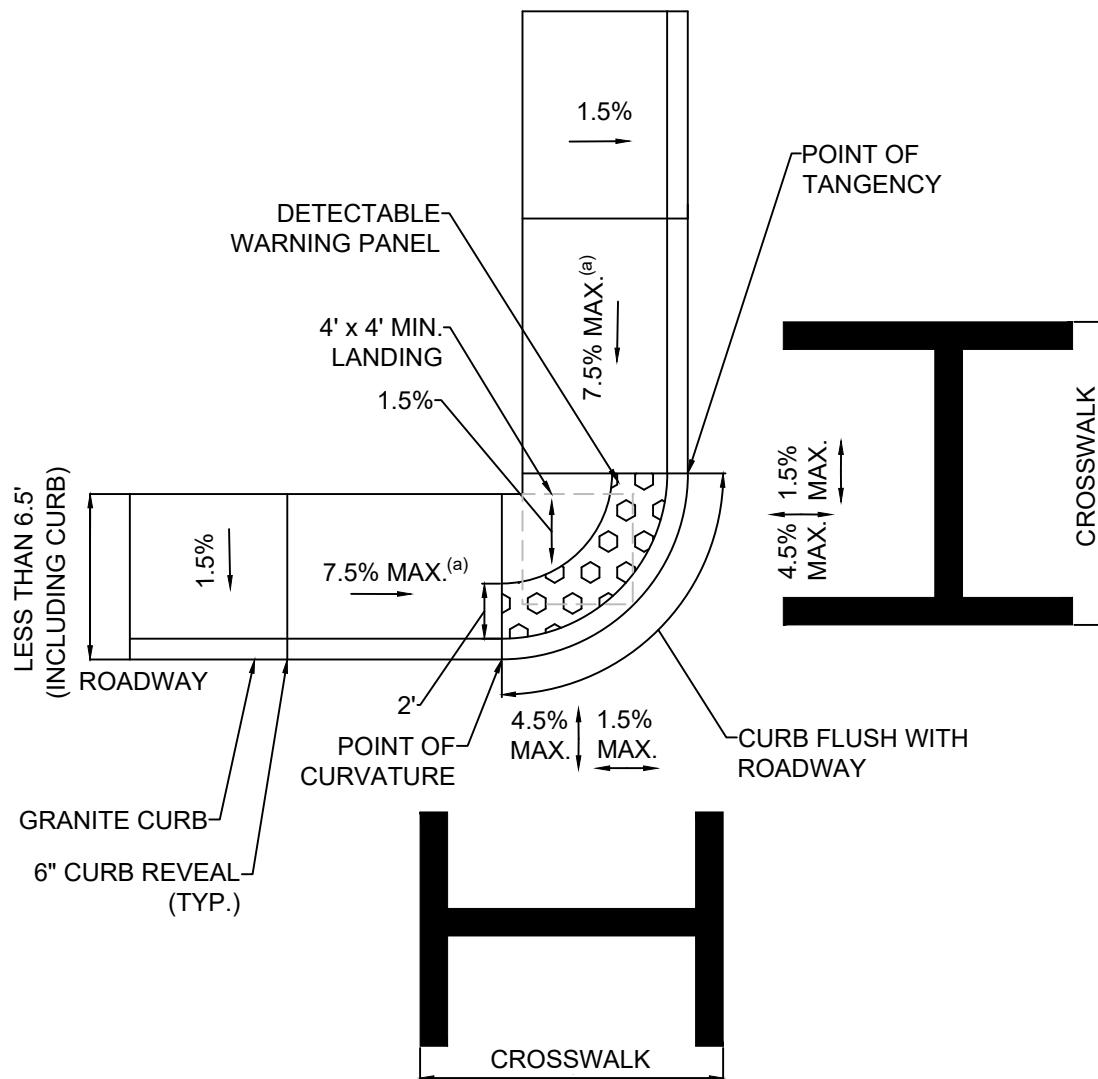
**BLENDED TRANSITION FOR TWO
CONTINUOUS DIRECTIONS OF TRAVEL
WITH SIDEWALK WIDTH OF 6.5' OR
GREATER (CASE 1)**

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R.11



NOTES:

1. CASE 2 - DEPRESSED CORNERS SHALL ONLY BE PERMITTED FOR RETROFITTING PROJECTS WHERE EXISTING ROADWAY CONSTRAINTS MAKE UNI-DIRECTIONAL COMPLIANT RAMPS INFEASIBLE.
2. (a) MAXIMUM SLOPE TRANSITION LENGTH MEASURED ALONG THE FACE OF CURB SHALL BE 15 FEET. A TRANSITION SLOPE GREATER THAN 7.5% IS ONLY ACCEPTABLE WHEN THE TRANSITION STONE REACHES A MAXIMUM LENGTH OF 15 FEET.
3. $\pm 0.5\%$ MAX. CONSTRUCTION TOLERANCE PERMITTED ON ALL SLOPES.
4. CORNER RADII SHOULD BE DESIGNED TO THE MINIMUM DIMENSIONS NECESSARY FOR THE DESIGN VEHICLE.



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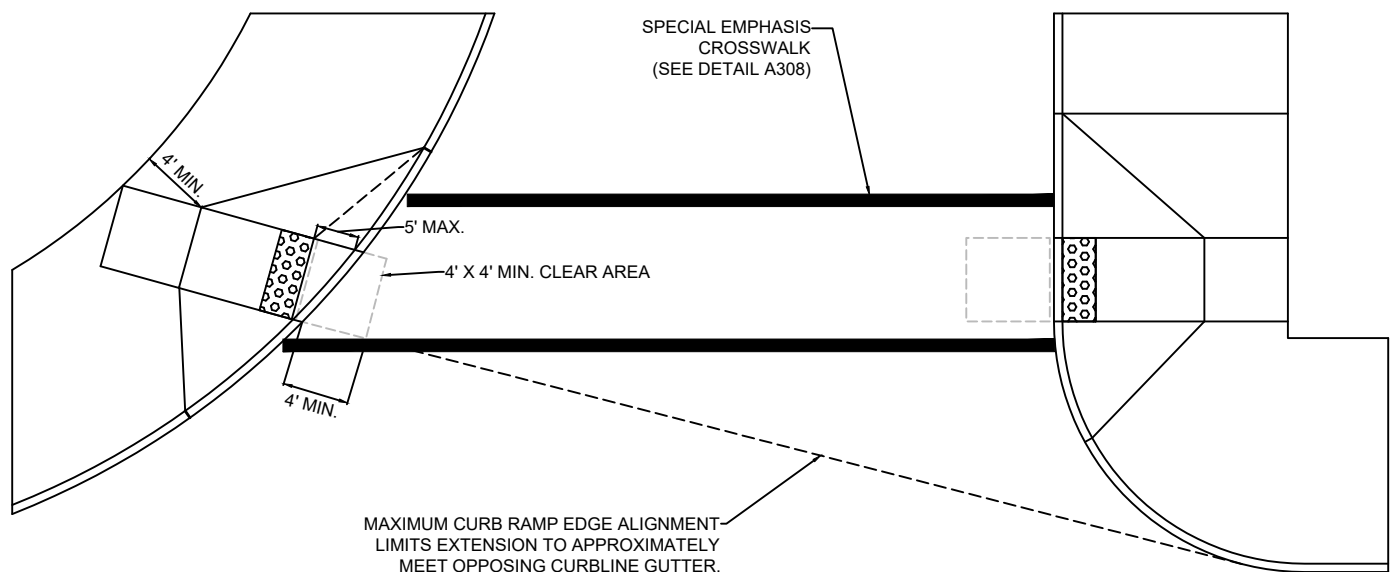
**BLENDED TRANSITION FOR TWO
CONTINUOUS DIRECTIONS OF TRAVEL
WITH SIDEWALK WIDTH LESS THAN 6.5'
(CASE 2 - DEPRESSED CORNERS)**

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R.12



NOTES:

- 1) TO THE MAXIMUM EXTENT POSSIBLE WHILE PROVIDING A COMPLIANT DESIGN, PEDESTRIAN CURB RAMPS SHALL RUN PARALLEL TO CROSSWALK MARKINGS, CENTERED WITHIN THE CROSSWALK, AND ALIGNED WITH PEDESTRIAN CURB RAMPS AT OPPOSITE SIDES.
- 2) THE DESIGNER SHOULD TAKE CARE TO BALANCE RAMP ALIGNMENT, DIRECTIONALITY, AND PEDESTRIAN DESIRE PATHS WHILE PROPOSING A COMPLIANT RAMP.
- 3) ANY ALTERATIONS TO OR DEVIATIONS FROM THIS GUIDANCE SHOULD BE DISCUSSED WITH AND APPROVED BY THE CITY



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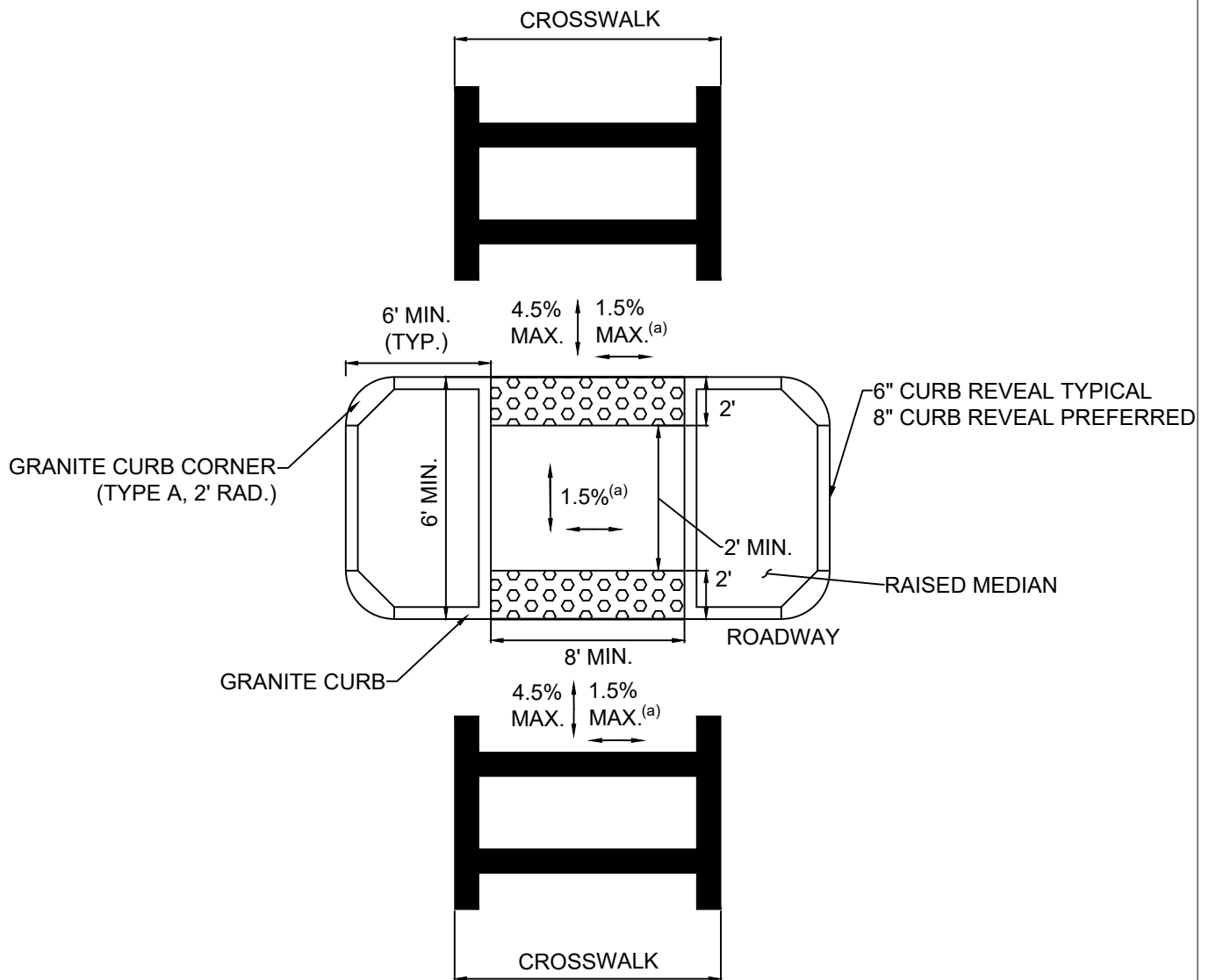
**MAXIMUM PEDESTRIAN CURB
RAMP ALIGNMENT LIMITS**

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DETAIL NO.

R.14



NOTES:

1. ^(a) THE LANDING CROSS SLOPE (PARALLEL TO THE CURB LINE) AND THE GUTTER RUNNING SLOPE SHALL BE A MAXIMUM OF 1.5% AT APPROACHES UNDER STOP OR YIELD CONTROL. THE RUNNING SLOPE MAY EXCEED 1.5% MAXIMUM UNDER THE FOLLOWING CONDITIONS:
 - SIGNAL OR PEDESTRIAN HYBRID BEACON= 4.5% MAX.
 - UNCONTROLLED = 4.5% MAX.
 - MIDBLOCK = NO GREATER THAN STREET GRADE
 - ROUNDABOUT = NO GREATER THAN STREET GRADE
3. $\pm 0.5\%$ MAX. CONSTRUCTION TOLERANCE PERMITTED ON ALL SLOPES.



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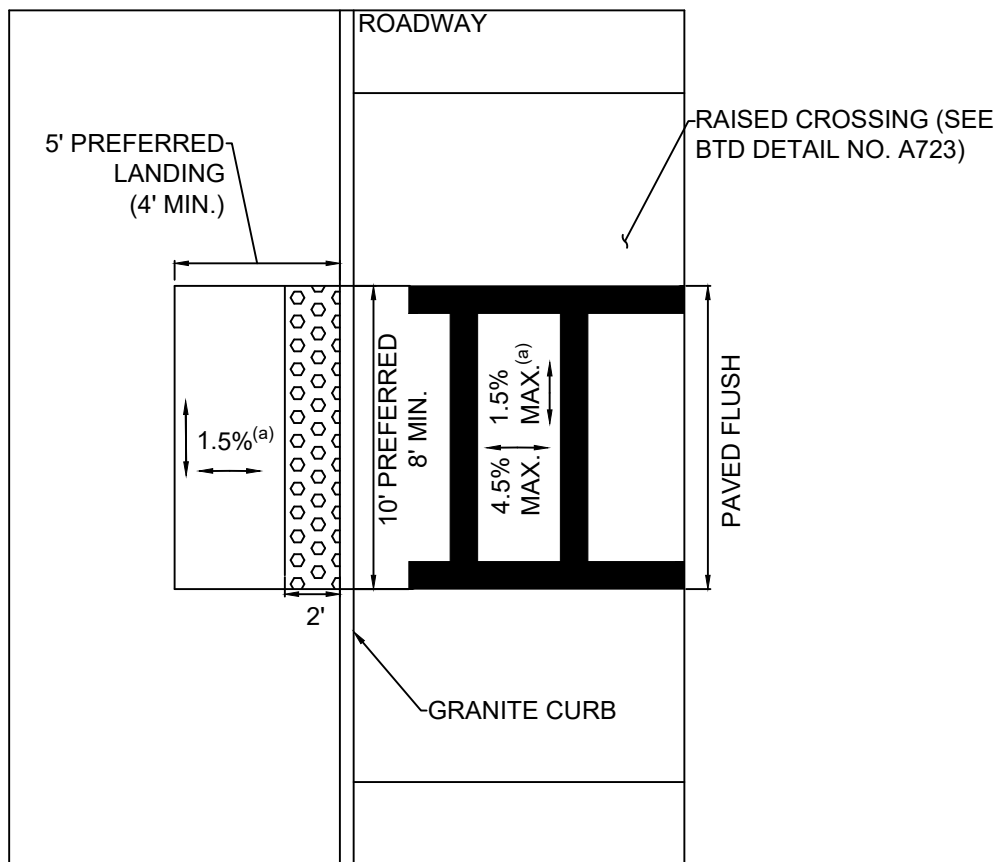
PEDESTRIAN REFUGE ISLAND

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R.15



NOTES:

1. $\pm 0.5\%$ MAX. CONSTRUCTION TOLERANCE PERMITTED ON ALL SLOPES.
2. ^(a) THE LANDING CROSS SLOPE (PARALLEL TO THE CURB LINE) AND THE GUTTER RUNNING SLOPE SHALL BE A MAXIMUM OF 1.5% AT APPROACHES UNDER STOP OR YIELD CONTROL. THE RUNNING SLOPE MAY EXCEED 1.5% MAXIMUM UNDER THE FOLLOWING CONDITIONS:
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 - UNCONTROLLED = 4.5% MAX.
 - MIDBLOCK = NO GREATER THAN STREET GRADE
 - ROUNDABOUT = NO GREATER THAN STREET GRADE



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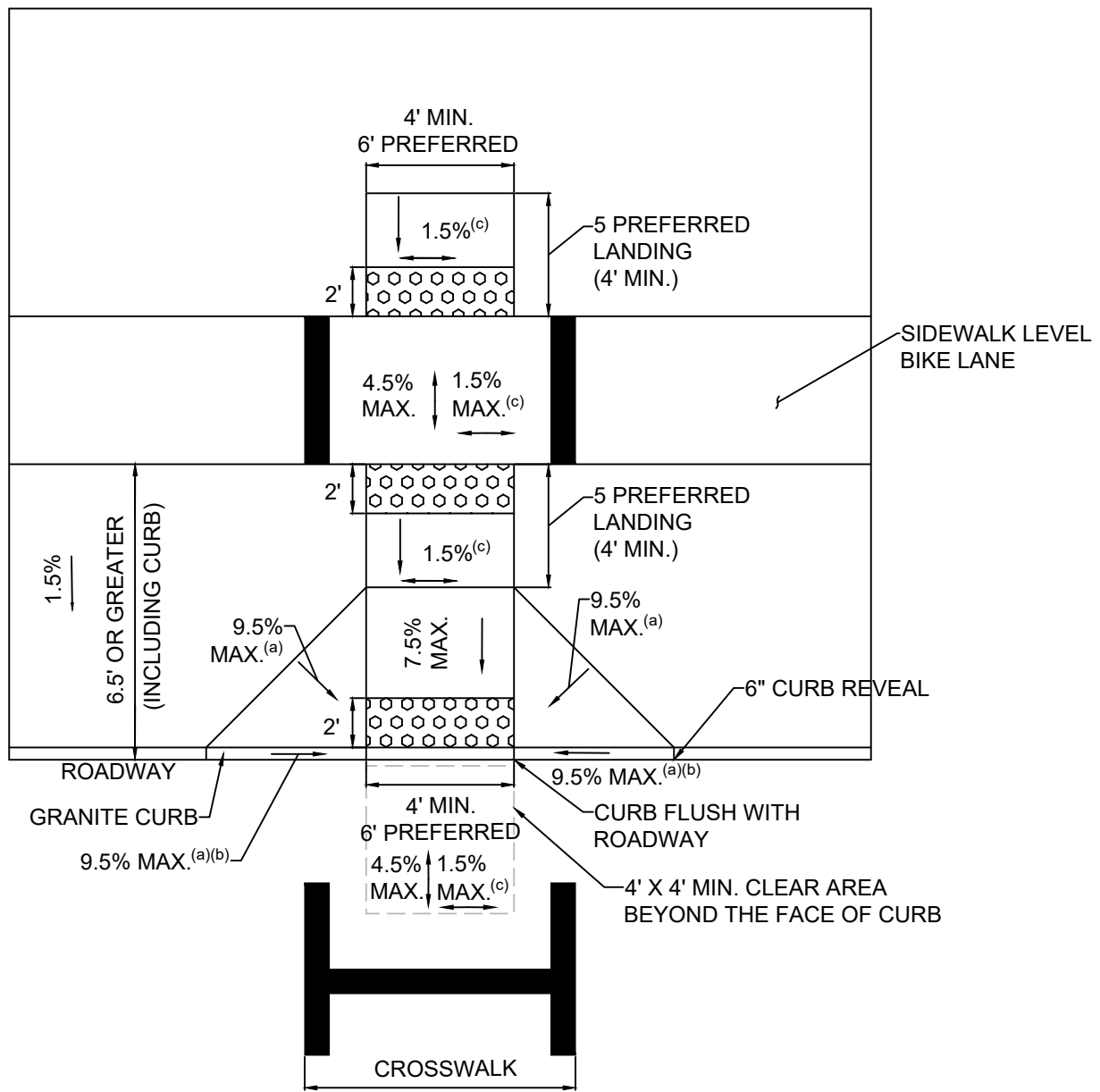
RAISED CROSSING RAMP

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R.16



NOTES:

1. (a) 7.5% PREFERRED SLOPE.
2. (b) MAXIMUM SLOPE TRANSITION LENGTH MEASURED ALONG THE FACE OF CURB SHALL BE 15 FEET. A TRANSITION SLOPE GREATER THAN 9.5% IS ONLY ACCEPTABLE WHEN THE TRANSITION STONE REACHES A MAXIMUM LENGTH OF 15 FEET.
3. (c) THE LANDING CROSS SLOPE (PARALLEL TO THE CURB LINE) AND THE GUTTER RUNNING SLOPE SHALL BE A MAXIMUM OF 1.5% AT APPROACHES UNDER STOP OR YIELD CONTROL. THE RUNNING SLOPE MAY EXCEED 1.5% MAXIMUM UNDER THE FOLLOWING CONDITIONS:
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 - UNCONTROLLED = 4.5% MAX.
 - MIDBLOCK = NO GREATER THAN STREET GRADE
 - ROUNDABOUT = NO GREATER THAN STREET GRADE
4. $\pm 0.5\%$ MAX. CONSTRUCTION TOLERANCE PERMITTED ON ALL SLOPES.



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PEDESTRIAN CURB RAMP ADJACENT TO RAISED BIKE LANE

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